

## PETITION & LOCAL MEMBER OBJECTION

COMMITTEE DATE: 19/12/2018

APPLICATION No. 18/02078/DCH DATE RECEIVED: 03/09/2018

ED: LLANDAFF

APP: TYPE: Full Planning Permission

APPLICANT: Ms Jenkins

LOCATION: 14 High Street, Llandaff, Cardiff, CF5 2DZ

PROPOSAL: ALTERATIONS TO PROVIDE HARD SURFACE, GATES AND REBUILD WALL PILLARS

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**RECOMMENDATION:** That planning permission be **GRANTED** subject to the following conditions:

- 1 C01 – Statutory Time Limit
- 2 The development, other than expressly detailed at conditions 3 and 4, shall be carried out in accordance with the approved drawing numbered A102.  
Reason: To ensure satisfactory completion of the development and for the avoidance of doubt in line with the aims of Planning Policy Wales to promote an efficient planning system.
- 3 Prior to the commencement of the development, hereby approved, notwithstanding the approved drawing details of the extent of the hard surface and its finish, which shall precisely confirm the areas of gravel, shall be submitted to and approved by the Local Planning Authority and the development shall thereafter accord with the approved details.  
Reason: To ensure the character of the Llandaff Conservation Area is preserved in accordance with Policy EN9 of the Local Development Plan.
- 4 Prior to commencement of the development, hereby approved, a planting schedule shall be submitted to and approved by the Local Planning Authority and planting shall be undertaken in accordance with the approved details during the first planting season following implementation of the development. Any plants which within a period of five years from completion of the development, die or are removed or become seriously damaged or diseased, shall be replaced in the next planting season with others of similar size and species.  
Reason: To ensure the character of the Llandaff Conservation Area is preserved in accordance with Policy EN9 of the Local Development Plan.

## 1. **DESCRIPTION OF PROPOSED DEVELOPMENT**

- 1.1 This application was deferred by Planning Committee at its meeting on 21<sup>st</sup> November 2018, in order to undertake a site visit. The visit took place on Monday 10<sup>th</sup> December, 2018 and the application is reported back for determination.
- 1.2 The application seeks planning permission to provide a hard surface, gates and rebuild wall pillars to the front of a dwelling house.
- 1.3 An existing low stone wall has been cut back to provide a 3.5m wide opening with re-constructed pillars at either side. It is proposed to reinstate the original pillar capping and install a 1.4m tall black painted wrought iron sliding gate within the opening.
- 1.4 It is proposed to provide a central path section of salvaged flagstones up to the front door of the dwelling, and the areas to both sides of the path to consist of loose buff gravel on a hardcore and stabilisation grid base. New landscaping is proposed in addition to retaining existing around the perimeter of the hard surface, and a condition is proposed to ensure details are agreed prior to the installation of the hard surface should consent be approved. The hardcore base of the hard surface has already been partially laid. A supporting statement has been submitted with the application.

## 2. **DESCRIPTION OF SITE**

- 2.1 The site comprises a two storey dwelling on the north-west side of High Street, immediately adjoining the Butchers Arms public house, and the Llandaff Institute Social Club. The front elevation of the public house is positioned at the back edge of the pavement in line with the front wall of the application site, obscuring the dwelling and its front garden from view when approaching from the south west. The social club is set back from the pavement aligned with the main front wall of the application dwelling but incorporates a raised hard surfaced external drinking area, enclosed by railings and a high ivy covered common boundary wall shared with the application site. The site is not prominent in the context of the wider street scene, but is located within the Llandaff Conservation Area. The building is included on the Council's list of buildings of local merit.

## 3. **SITE HISTORY**

- 3.1 14/01655/DCH – planning permission granted and implemented for a two storey rear extension.

## 4. **POLICY FRAMEWORK**

- 4.1 Relevant National Planning Guidance:

Planning Policy Wales (Edition 10, 2018)  
Technical Advice Note 12: Design

4.2 Relevant Cardiff Local Development Plan (2006-2026) policies:

Policy KP5 (Good Quality and Sustainable Design)  
Policy EN9 (Conservation of the Historic Environment)  
Policy T5 (Managing Transport Impacts)

4.3 Relevant Supplementary Planning Guidance

Residential Extensions & Alterations (2017).  
Llandaff Conservation Area Appraisal (2006).

5. **INTERNAL CONSULTEE RESPONSES**

5.1 Transportation – highways have licensed the installation of a crossover at the above property, as detailed in the planning application, and that the finished works have now been inspected. Inspection of the works has confirmed that the crossover has been installed in accordance with the license as far as dimensions and kerb interface with the carriageway are concerned. The submission has been assessed and is considered to be acceptable, the provision of on-plot parking is policy compliant and use of the proposed crossover is not considered to represent a risk to other road users. I therefore have no objection to the application as submitted.

5.2 Highways & Parks (Drainage) - no formal objection to the principle of the application as the increase in impermeable area being negligible.

6. **EXTERNAL CONSULTEE RESPONSES**

6.1 Llandaff Society – object to the application on the grounds that it conflicts with Local Development Plan and Conservation Area policy and would neither conserve nor enhance Llandaff Conservation Area. The Society is strongly of the view that: (i) work undertaken in advance of obtaining permission for use of a small front garden in the heart of a Conservation Area for parking cannot be used to justify obtaining permission for that use on the basis that it would tidy up the mess created. (ii) ownership of a locally listed property within the oldest and – we suggest – the most significant Conservation Area in Cardiff confers responsibilities to conserve and enhance the property and its setting for general community benefit. (iii) inaccurate dimensions, and lack of consideration of the porch, in this application (and that for the cross-over) suggest that it does not comply with the Council's requirements for use as a vehicle hardstanding - even if there weren't objections on other grounds, including its location in a Conservation Area; (iv) in relation to points made by Ms Jenkins in support of this planning application: (a) discussions with Officers do not tie decision-makers hands; (b) permission for a cross-over was obtained based on misleading information (see (iii) above) which was clearly not checked before permission for that facility was granted. (c) re para 1.1 in her covering letter, a sliding gate as proposed can clearly not "match the design of the original" small garden gate. By using inappropriate comparisons with properties that open directly onto High Street she does not appear to

understand the significance of the street pattern, nor the importance of her property and its garden, stone wall, pillars and pedestrian gate as an integral part of the character of Llandaff. (d) re her para 1.3, Myrtle Villa was not a “forgotten” house with a “forgotten” front garden. It was much loved by its former owners - long-term occupants Mrs Caruana and her sister (unfortunately now both deceased) - and by residents and visitors. Ms Jenkins bought the property knowing that it was in a Conservation Area and had no parking. Previous work undertaken is not relevant to whether the current proposal should be approved. (e) re para 2 Ms Jenkins cites as local precedents numerous cases which are either new builds, which have been subject to scrutiny, or those established before 1968 when the Conservation Area was designated. No: 51 Cardiff Road is not comparable because the garden directly in front of the house was not affected by the new driveway and parking. (f) re para 3.5 parking enforcement (or lack of it) is no justification for further exacerbating the deplorable conditions re parking on the High Street. (g) re para 4.2 we want to make it clear that Llandaff Society has not been approached by Ms Jenkins or her agents to discuss this matter. (h) para 5.1 appears to be a direct threat to retain the eyesore of a half-demolished front garden and should be given no weight in the decision. (i) we are convinced that conserving and enhancing Llandaff Conservation Area is of the utmost importance as a jewel in the crown of Cardiff in terms of its visitor appeal and tourism offer. To allow a substandard proposal - which would detract significantly from that jewel - would be a travesty.

## 6.2 Llandaff Conservation Group – object for the following reasons:

- a) The application is to remove a small front garden to replace it with car parking immediately and closely in front of the front door and windows of a terraced dwelling house in a prominent location in the heart of the Llandaff Conservation Area. This is a small location. There is insufficient space if used for car parking for a hinged gate as previously, and a sliding, double vehicular access gate is proposed in the stone walling. This proposal if permitted will adversely affect the conservation area by disrupting the conserved quality of Llandaff High Street, creating unsightly parking between the property and the carriageway, thereby obscuring the front and spoiling the view of locally listed properties. It will also distract from the quality of the stone walling that is a noted feature of the Llandaff Conservation Area.
- b) This proposal is a significant departure from the existing situation. There is no other such arrangement in the High Street, there being no other dropped kerb leading to a single dwelling house for the parking of an individual vehicle. The applicant suggests there are nine dropped kerbs in High Street. However, dropped kerbs in this area include pedestrian provision put in place to support the carefully created pedestrian environment which is a result of significant Council investment in the High Street. There are only two vehicular dropped kerb provisions and they are entirely different in style and purpose from that now proposed. One leads to a small street area containing businesses and housing and the other is access to the doctor’s surgery building. Importantly, these locations can accommodate numerous vehicles, unseen from the High Street and with

suitable space to turn and enter and exit the High Street always moving forward with suitable vision. This application at 14 High Street is not comparable.

- c) No other single house or dwelling has unique dropped kerb access for vehicles over the pavement of the High Street. There is no reason now to disrupt this pattern of the conservation of frontages of the High Street to provide parking for one property alone and which is located amongst attractive locally listed buildings. Other dwellings and houses such as those on the opposite side of the High Street shops have to make parking arrangements elsewhere as has this property previously.
- d) This location is an important part of Cardiff's Conservation Areas being the road to Llandaff Cathedral. It is a general visitor area and highly visible and prominent when televised in use for significant all Wales or Cardiff occasions for example when used by visiting dignitaries such as the Queen and members of the royal family. To allow this particular single parking provision to impact disproportionately on the conserved High Street of the City of Llandaff area would notably detract from the conservation area. It would be a poor planning decision in relation to other Council initiatives and significant investment within the last fifteen years to enhance and promote the conserved High Street area and its pedestrian environment.

#### Other matters of concern

'Precedent' as stated in the application:

The Llandaff Conservation Area Advisory Group is aware that all applications are judged on their merit but is highly concerned at the references to 'precedent' and the following statement made in the application: *"Nearly all houses and properties in the Llandaff Conservation Area have already replaced their front lawns for hard surface frontages/forecourts, driveways or parking bays, and as such resident/customer practice of parking at the front of their houses and properties is long established in the area."*

This is not correct and is particularly misleading when stated in relation to terraced dwelling houses with frontages of a similar nature to 14 High Street. There are numerous walled small front gardens in Llandaff Conservation Area that have not been converted to car parking. Notably these are terraced properties on Cathedral Green, Bridge Street and Cardiff Road. Size of frontages vary but are equivalent or larger than 14 High Street in many instances. If other properties are permitted to convert their frontages to car parking in the same manner as proposed in this application a major quality of the Llandaff Conservation Area would be destroyed. Article 4 provision and Conservation Area status has previously been used to prevent or discourage such development.

Photographs submitted with the application:

The Conservation Area Advisory Group notes the photographs provided are described as precedent photographs in support of the application. The Advisory Group seriously believes they demonstrate the case *against* the application by showing how where parking for dwellings has been permitted or established in the Llandaff Conservation Area it has always been distinctly in

situations and properties entirely and critically different from 14 High Street, in locations providing far greater space and vehicle safety and less adverse impact on well conserved locations. *Specifically:*

On Cardiff Road: Number 51 is offered as a comparable situation. But it is a large entrance accessing garages and a substantial car park capable of accommodating several vehicles with turning space and is not a terraced house front. Entrances to public properties such as to the St Michael's College car parking area, and buildings and Cathedral School Nursery buildings are also indicated. Also identified on the Cardiff Road aerial photographs are large commercial concerns such as the Pritchard Court and Pegasus retirement home sites with purpose-built carparks and traffic flow. These examples do not present comparable circumstances to 14 High Street or present the same negative conservation or vehicle safety issues.

On Cathedral Green: Older cathedral related houses (the notable 'white' buildings) are shown with long established parking inside the curtilage of the sites. These are enclosed spaces with suitable room to move vehicles safely, drive out forwards safely and to shield vehicles from significant public view without masking buildings of note, sometimes also including provision of matched garages. These do not present comparable circumstances to 14 High Street or have the same negative conservation or safety issues.

On Cathedral Close: Nos 6 and 8 were built in the 1960's. Their parking areas and integral garages were in the original plans and are entirely appropriate. Large dwelling houses are shown that have significant space to allow for vehicle access and egress for properties. These do not present comparable circumstances to 14 High Street or have the same negative conservation or safety issues.

On Bridge Street: Spencer's Row (effectively a Victorian street of eight houses with full vehicular access and egress) and the Old Police Station together with Pen Pentre House having substantial enclosed off-road parking is pictured, as are various comparatively modern properties with long driveways built to include parking alongside them. A mix of very different locations, once again these do not present comparable circumstances to 14 High Street or have the same negative conservation issues. The Old Mason's Arms is an end terrace property, a former public house converted to housing and adjacent to modern detached housing allowing greater vision for moving vehicles. It does have an enclosed front parking space big enough to include hinged gates, believed pre-Article 4 provision, pictured without a vehicle in it. When viewed, especially when containing a vehicle, this does arguably detract significantly from the conserved quality of the rest of Bridge Street where the frontages all remain and where further such developments have not been permitted or taken place. Such development has a negative impact here and if similar is now located in a more prominent position in the conservation area such as 14 High Street it will have an even greater negative conservation impact.

On Bridge Road: This area of the Llandaff Conservation Area pictured contains mixed twentieth century housing built with garages to the rear and off-road parking. This built environment does not present relevant circumstances to 14 High Street in the heart of the conservation area or in the specific context of this application.

Photographs presented of the interior renovations at 14 High Street do not offer material considerations in relation to this application regarding impact on the surrounding conservation area. They should not inform this application. The building is not a listed building so interior adjustments are not material consideration in this application.

Photographs of garage door vehicular access showing rear or side access in Chapel Street show ugly provision of other examples using sliding doors that disrupt the stone wall environment and detract from the conservation area. They are significantly different from the frontage of a terraced house on the High Street.

Road Safety and the Crossover Terms and Conditions:

The applicant states the house location as being on '*an unclassified no through road*'. This refers to Llandaff High Street which is a very busy two-way road that is the main access route to the cathedral and heavily used daily by large numbers of vehicles and pedestrians. These include cars, heavy lorries making deliveries to food shops, cafes, pubs and restaurants on High Street and to houses on the Cathedral Green. The road regularly carries coaches visiting the cathedral and also funeral corteges. Vehicles enter High Street and can exit by turning in the Cathedral Green area and back through the High Street or by turning off to use the roundabout adjacent to the large Council Pay and Display car park serving the area which is also accessed and egressed through Llandaff High Street. Vehicles can also enter the High Street from the Cathedral Green having accessed it by Heol Fair, the street that is parallel to the High Street which provides access to the Cathedral Green from Cardiff Road and then on to the High Street. Llandaff High Street is without any doubt a very busy location indeed with significant traffic input from both ends passing through a pinch point passing in front of 14 High Street, the site of the proposed car standing. There is often serious traffic congestion. There is high pedestrian usage in this location to access the varied shops and food premises but also to make pedestrian access to the Cathedral School, Bishop's Palace Gardens and in particular, the new Memorial Hall. This is used as part of the school in the day and receives high evening use by the community.

In the context of the above: The Llandaff Conservation Area Advisory Group feels it is likely to be an unwise and unsafe decision to allow and promote the vehicle movements that will result from this parking space in this specific location. The Group calls on the Highways Authority to confirm it has undertaken, and also to present publicly, the proper risk analysis conducted in relation to this permission and without which it would be unsafe to proceed.

It is clear there is limited space, limited visibility ('vision splay') in this location and it is probable that vehicles parking in the space are likely to reverse in or out of the standing from time to time. The Group notes that the Crossover Terms and Conditions agreed in relation to this application specify:

- *Crossings are approved on the basis that vehicles will drive out forward onto the highway.*
- *The maximum width of a single crossover is 3 metres. (Minimum is 2.4m) The maximum width of a double crossover is 4.8 metres. The depth of the forecourt must be at least 4.8 Metres, in order to ensure that vehicles do not overhang onto the public footway. Vehicles overhanging onto the footway are liable for prosecution.*
- *If you have a porch, bay windows, or steps to your front door, will need to be considered in the space available to park your car. Also the position of your front door and if space is available to the side of your house within your property may need to be considered in your application.*

It is clearly apparent that there is very limited space and limited visibility ('vision splay') in this location at 14 High Street and it is likely that vehicles parking in the space will reverse in or out of the frontage, over the pavement, from time to time. The Group calls on the Council to confirm it has thoroughly checked all dimensions and practicalities in relation to the above required terms and conditions and to ensure that any building development permitted will safely ensure they must be properly observed.

Measurement of the scaled drawings with the application suggests the distance between the edge of the porch to the footway is 4.2 metres which is less than the 4.8 required. The proposed planting, indicated to attempt to reduce the negative visual impact of the development, would also reduce the total space further. The document A104 - Proposed View submitted to support the application gives an entirely false impression of the size of the space and the way in which vehicles could be parked facing the building. This must be addressed with full and proper consideration.

## 7. **REPRESENTATIONS**

7.1 The application was publicised by neighbour letter, site and press notices. A petition of objection was received, signed by 61 residents with addresses in Cardiff, all of which could reasonably be affected by the matter when passing the application site. Full details are viewable online.

7.2 Objections have been received from the following addresses:

- no. 4 Pendinas, The Cathedral Green.
- no. 33 Cardiff Road.  
Full details are viewable online, their comments are summarised as follows:
  - a) The works already and illegally performed have been done shoddily. To allow them to remain makes a mockery of conservation.*
  - b) To permit this application would set a precedent for development of all Cathedral Green's frontages.*
  - c) Damaging to the amenity of the conservation area;*
  - d) Hazardous to pedestrians;*
  - e) The Llandaff Conservation area was the first to be designated in Wales;*



f) *Myrtle Cottage is of local importance which could justify an official listing.*

7.3 Cllr Phillipa Hill-John objects to the application as follows:

*I would also like to request the opportunity for a site visit in order for the planning committee to appreciate the context of this proposal. It should be noted that there is considerable local interest in this case, and I am aware both the Llandaff society and the Conservation group for Llandaff have also submitted their objections. Without repeating all the points the respective groups have raised I would like to endorse their objections and have recorded particular points of concern.*

*Road Safety and the Crossover Terms and Conditions:*

*I would like to request that the council confirm that they have conducted a full and detailed assessment of the crossover and that the proposal meets all requirements, in terms of measurements, dimensions, (Measurement of the scaled drawings with the application suggests the distance between the edge of the porch to the footway is 4.2 metres which is less than the 4.8 required.) Also the Terms and Conditions for Crossovers as noted by the conservation group and noted again below:*

- Crossings are approved on the basis that vehicles will drive out forward onto the highway.*
- The maximum width of a single crossover is 3 metres. (Minimum is 2.4m) The maximum width of a double crossover is 4.8 metres. The depth of the forecourt must be at least 4.8 metres, in order to ensure that vehicles do not overhang onto the public footway. Vehicles overhanging onto the footway are liable for prosecution.*
- If you have a porch, bay windows, or steps to your front door, will need to be considered in the space available to park your car. Also the position of your front door and if space is available to the side of your house within your property may need to be considered in your application.*

*The property faces onto a busy high street with high levels of pedestrians, school children, visitors to the area and the visibility is poor with the requirement - as set put in the conditions - for the vehicle to reverse into Parking area will be difficult. All aspects relating to the safety need to be considered prior to any application being approved.*

*Planning conditions:*

*It should also be noted that article 4(2) direction that was presented to Cabinet in July was as a direct result of the demolition of this walled enclosure, Which clearly states - 'that planning permission will be required to carry out ..... any planning operation consisting of the demolition of the whole or part of any gate, fence, wall or other means of enclosure .....' Whilst I respect that this cannot be applied retrospectively it certainly needs consideration, given that such conditions are now in place to stop such activity and to conserve what is becoming an increasingly altered landscape in the conservation area.*

*Conservation area:*

*By allowing this proposal, it will adversely affect the conservation area by disrupting the conserved quality of Llandaff High Street, creating unsightly parking between the property and the carriageway, thereby obscuring the front and spoiling the view of locally listed properties. It will also distract from the quality of the stone walling that is a noted feature of the Llandaff Conservation Area and listed in the Conservation area appraisal. It will do nothing to enhance the frontage of a locally listed property of interest to have a vehicle parked directly outside that will obscure the remaining original features. No other property in the High St has its own unique dropped kerb arrangement and therefore why should this proposal be permitted. As noted by the conservation group – There are only two vehicular dropped kerb provisions and they are entirely different in style and purpose from that now proposed. One leads to a small street area containing businesses and housing and the other is access to the doctor's surgery building. Importantly, these locations can accommodate numerous vehicles, unseen from the High Street and with suitable space to turn and enter and exit the High Street always moving forward with suitable vision. This application at 14 High Street is not comparable.*

*In light of careful consideration for Road safety, compliance to Crossover Terms and Conditions, the Conservation Area Appraisal Review, the recent amendments to the Article (4)2 direction I would seek to request that the planning committee consider this application very carefully.*

- 7.4 A petition of support for the proposal was received, signed by 39 persons with no addresses specified. Full details are viewable online. As no addresses are specified it does not demonstrate that the named persons could be affected by the matter and contains less than 50 signatures, therefore is not valid.

8. **ANALYSIS**

- 8.1 The key issues are the effect of the proposal upon the character of the Conservation Area, highway safety and the setting of a locally listed building.

8.2 **Highway Safety Considerations**

In respect of highway safety, Highways have confirmed that the vehicular crossover has been licensed and is not considered to represent a risk to other road users.

8.3 **Design Considerations**

In respect of the effect upon the Conservation Area the Llandaff Conservation Area Appraisal notes the contribution made by boundary walls and gates to the character of the area: *There are a variety of ways in which verge gardens and footpaths have been formed. In repairing and restoring boundaries or hedges, it is important to plant a species in character. Tree Preservation Officers may be able to advise on species suitable for your particular situation. Whilst hedges become established a temporary post and wire or mesh fence*

*may be an acceptable method for providing security to a garden. Boundary walls should be regarded as an extension of the building which it protects. In Llandaff they define ownerships, provide privacy and often identify the age and character of the buildings they enclose, or their predecessors. Most importantly, walls define space and provide definition and character to the street scene. New walls should seek to fulfil the same objectives. There are a number of different building materials in Llandaff and a huge variety of stone. Where natural stone walls are retained they should be repaired and maintained to ensure that stone work, joints, mortars and copings all respect the original structure. Black painted railings on a stone base are also typical of Llandaff. They should be maintained and if necessary replaced, using profile and sections to match.*

- 8.4 The works to partially demolish the front boundary wall were undertaken in late 2017, and have been determined to be lawful by virtue of Class B of Part 31 of the General Permitted Development Order 1995 as there was no Article 4 (2) Direction in place removing the right to undertake such works within the Llandaff Conservation Area. Also, the works did not require Conservation Area Consent as the requirement for Conservation Area Consent is only applicable to total/substantial demolition of a means of enclosure. Having regard to the existing lawful situation with an open aspect between the retained sections of wall, the proposed re-introduction of an enclosure is considered to represent an enhancement to the character of the Conservation Area. The gate would be of traditional decorative design and its material and colour would match the previous gate. Original pillar capping would also be reinstated along with new mortar pointing to the stonework. Having regard to the existing lawful situation and proposed enclosure, it is not considered that the proposal would adversely affect the character of the locally listed building.
- 8.5 The Llandaff Conservation Area Appraisal also notes the contribution of front gardens to the character of the area: *Llandaff was largely developed before car ownership became widespread. Constructing further off-street parking within the curtilage of individual houses can result in the loss of front gardens and hedges. Where there is room for a garage, it should be designed in sympathy with the house to which it relates and without detriment to the overall area. Grassed and planted front gardens and continuous hedging help to give the Conservation Area its special charm. The need to preserve this character may preclude the grant of planning permission to form a hard-standing in the front garden. Each case will be considered on its merits. The location of a hard-standing should not result in cars being parked directly in front of the house. Many roadsides include a grass verge contributing to the green character of the area. These should be protected. The creation of paved forecourts with turning areas usually reduce grassed areas, flower beds and the line of hedging, and result in the loss of a simple garden with concrete path.*
- 8.6 While the creation of a hard surface would not be consistent with the guidance set out in the Conservation Area Appraisal, it is recognised that each case will be considered on its merits. In this respect, it should be noted that the Highways department have licensed the installation of the vehicular crossover

into the site. Furthermore, as set out in paragraph 8.4, the partial demolition of the front boundary wall is deemed lawful. Therefore, these unique circumstances have created a situation where vehicular access can be lawfully secured from the highway directly into the front curtilage.

- 8.7 The above context cannot be dismissed, as regardless of the decision upon this application and any potential enforcement, the owner would continue to retain the right to park a vehicle in the curtilage to the front of the dwellinghouse. Therefore, and while not strictly in accordance with the guidance as described above, the current proposal is considered to represent an opportunity to secure a high quality treatment to the front boundary enclosure of the front garden area. The central path section would be of salvaged flagstones presenting an enhanced appearance to the previous concrete path, and the areas at both sides would consist of loose buff gravel on a hardcore and stabilisation grid base. Landscaping would be provided around the perimeter.
- 8.8 It should be noted that the visual impact is limited in terms of the street scene. Views of the front garden from the south west are obscured by the adjoining building at no. 16 (Butchers Arms public house) which projects forward to the pavement, and views from the north east are partially obscured by a vegetation covered boundary wall with no. 10 (Llandaff Institute) and the view from this direction is dominated by the raised front forecourt of which consists of a hard surface of concrete paving.
- 8.9 It is not considered that the proposal would set a precedent for hard surfaces within the Llandaff Conservation Area. The application property is an isolated residential dwelling within a terrace of otherwise commercial buildings on the north west side of High Street. It should also be noted that an Article 4(2) direction has subsequently been served on 11<sup>th</sup> August 2018 to remove permitted development rights within the Llandaff Conservation Area for alterations to means of enclosures under Part 31 of the General Permitted Development Order 1995, thus preventing further partial demolition of front enclosures.
- 8.10 Representations

The representations received from Cllr Phillipa Hill-John, the Llandaff Society, the Llandaff Conservation Area Advisory group and local residents are noted and have assisted in the consideration of the matter.

- a) Impact upon the character of the Conservation Area: Having regard to the lawful widened opening created by the partial demolition of the original boundary wall as detailed in paragraph 8.2 it is considered that the proposed enclosure would not impact to an unacceptable degree on the preservation and enhancement of the Conservation Area as a whole. While noting the replacement of the original garden, the introduction of the hard surface as proposed, when taken into consideration with the enclosure and new gate, will not, it is considered unacceptably harm the character of the Conservation Area. The hard surface will incorporate

landscaping and a new path to the front door.

- b) Impact upon Highway Safety: It is considered the proposal would not have any adverse impact upon highway safety. The Council's Highways section have agreed the installation of the vehicular crossover, allowing vehicular access to the front curtilage with the property. The distance between the front elevation of the building and the front enclosure has been measured on site as 5 metres which confirms the scaled proposed plan drawing as accurate. Whilst the distance from the porch to the footway is less than the minimum of 4.8m as recommended by the Highways section to ensure vehicles do not overhang the footway, in this particular instance it should be possible to park at an angle without the vehicle overhanging the footway.

The representation in support of the application is noted.

#### 8.11 Other Legal Considerations

*Crime and Disorder Act 1998* – Section 17(1) of the Crime and Disorder Act 1998 imposes a duty on the Local Authority to exercise its various functions with due regard to the likely effect of the exercise of those functions on, and the need to do all that it reasonably can to prevent, crime and disorder in its area. This duty has been considered in the evaluation of this application. It is considered that there would be no significant or unacceptable increase in crime and disorder as a result of the proposed decision.

*Equality Act 2010* – The Equality Act 2010 identifies a number of 'protected characteristics', namely age; disability; gender reassignment; pregnancy and maternity; race; religion or belief; sex; sexual orientation; marriage and civil partnership. The Council's duty under the above Act has been given due consideration in the determination of this application. It is considered that the proposed development does not have any significant implications for, or effect on, persons who share a protected characteristic.

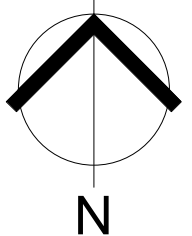
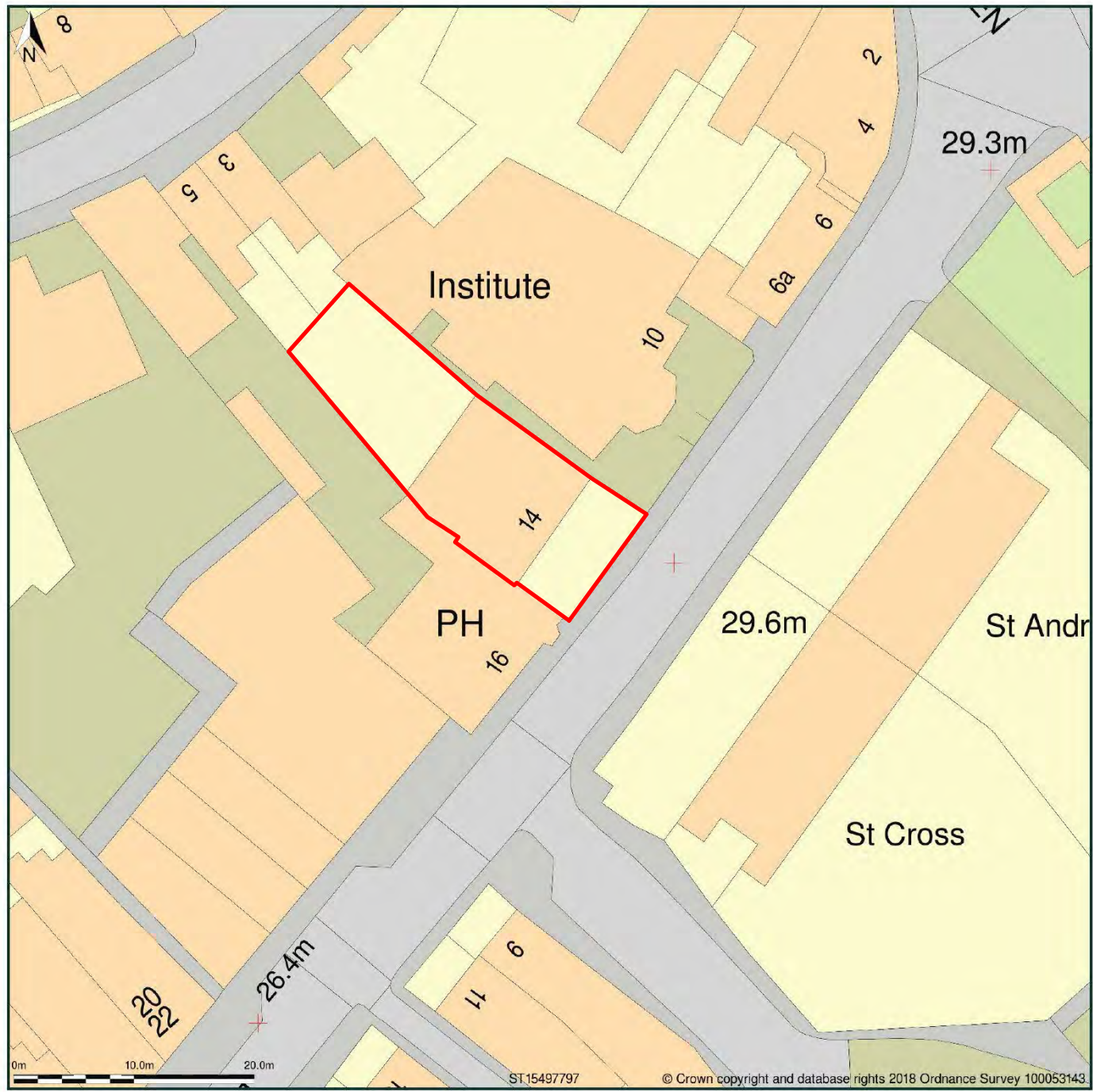
*Well-Being of Future Generations Act 2016* – Section 3 of this Act imposes a duty on public bodies to carry out sustainable development in accordance with the sustainable development principle to act in a manner which seeks to ensure that the needs of the present are met without compromising the ability of future generations to meet their own needs (Section 5). This duty has been considered in the evaluation of this application. It is considered that there would be no significant or unacceptable impact upon the achievement of wellbeing objectives as a result of the recommended decision.

#### 8.12 Conclusion

It is concluded that the application is acceptable in accordance with the planning policies listed, and is recommended that planning permission be granted, subject to conditions.

DO NOT SCALE FROM THIS DRAWING. IF IN DOUBT ASK!  
REFERENCE **MUST BE MADE** ALSO TO STRUCTURAL  
ENGINEER DRAWING INFORMATION WHERE RELEVANT  
TO PROJECT. S.ENGINEER INFO TAKES PRECEDENCE  
OVER STEEL/TIMBER SIZES INDICATED ON THIS  
DRAWING

14 High Street, Llandaff, Cardiff CF5 2DZ



### Location Plan

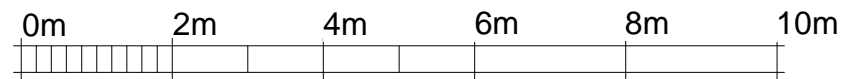
Alterations to provide hard surface, gates & re-build piers  
to No 14 High Street, Llandaff, Cardiff

A100

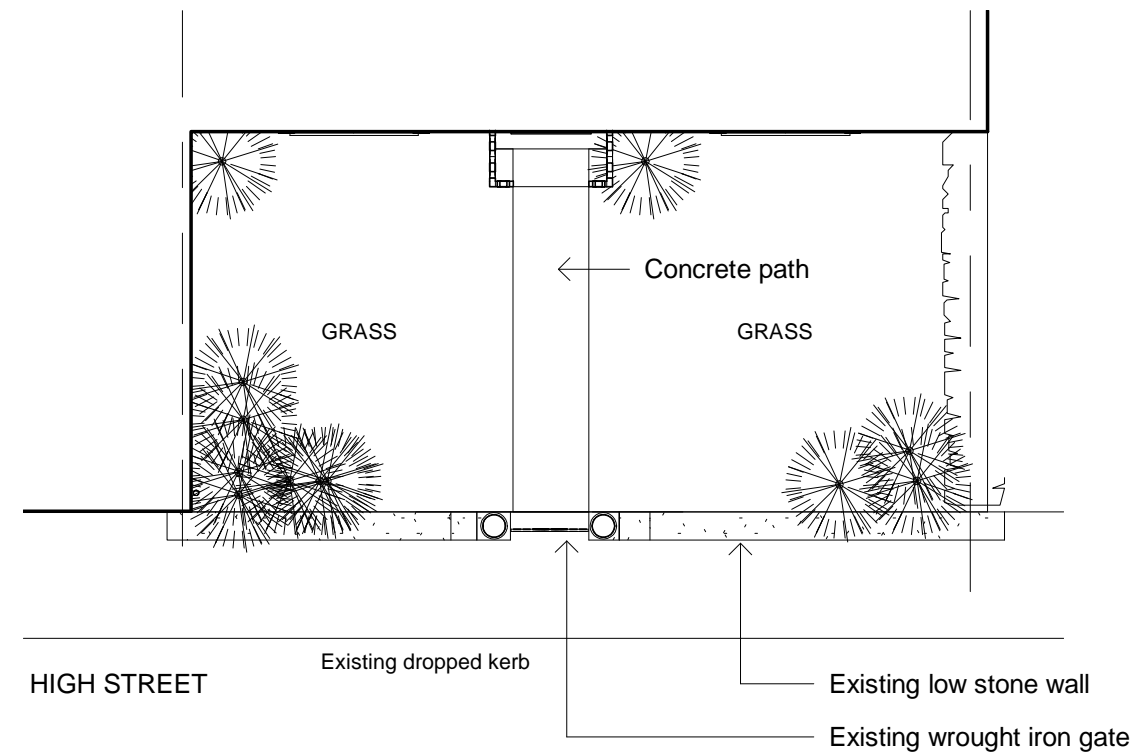
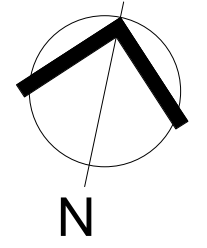
Project number	277
Date	MAR 2018
Drawn by	MD
Scale	1 : 500@A3

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 OVER STEEL/TIMBER SIZES INDICATED ON THIS  
 DRAWING



SCALE 1:100 @ A3



Original Elevation

## Existing Plan

1 : 100



Original stone wall demonstrating plant growth effecting joints and wall integrity



Further indication of plant growth that has penetrated the mortar joints in places



## Existing Elevation

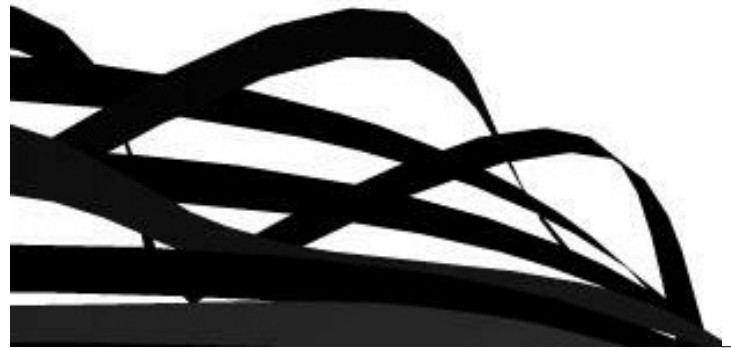
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Existing Plan & Elevation  
 Alterations to provide hard surface, gates & re-build piers  
 to No 14 High Street, Llandaff, Cardiff

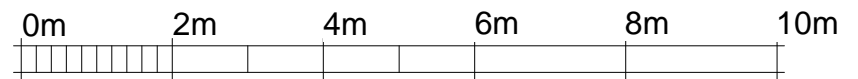
A101

Project number	277
Date	MAR 2018
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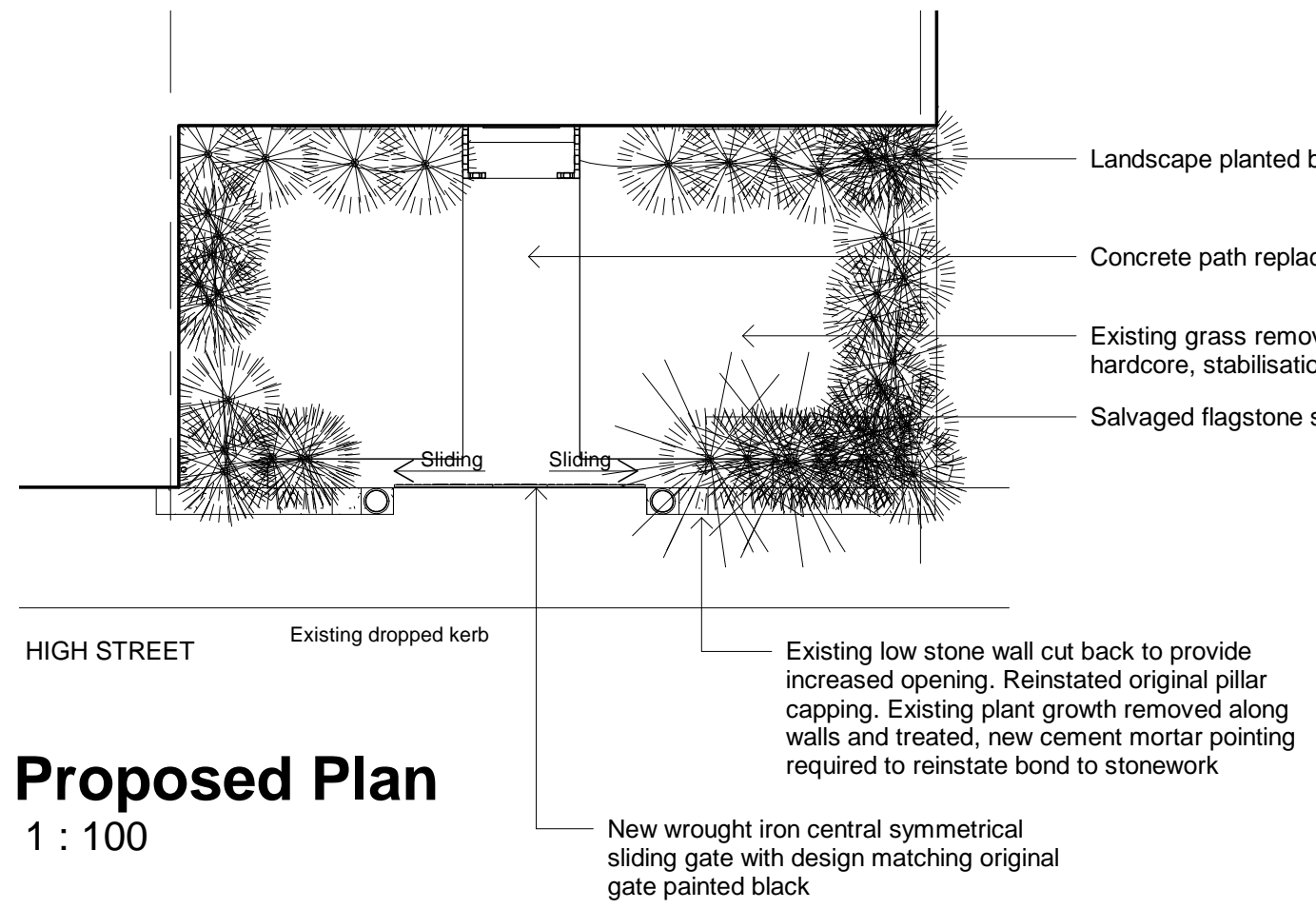
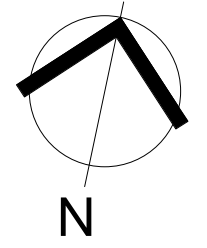
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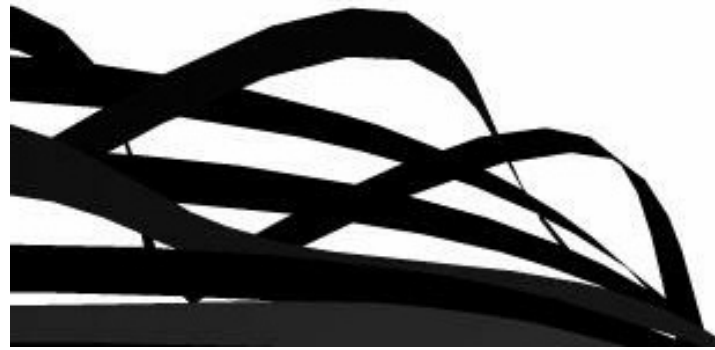
SCALE 1:100 @ A3



**Proposed Plan**  
1 : 100



**Proposed Elevation**  
1 : 100



Proposed Plan & Elevation  
Alterations to provide hard surface, gates & re-build piers  
to No 14 High Street, Llandaff, Cardiff

A102

Project number	277
Date	MAR 2018
Drawn by	MD
Scale	1 : 100@A3

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 ENGINEER DRAWING INFORMATION WHERE RELEVANT  
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 OVER STEEL/TIMBER SIZES INDICATED ON THIS  
 DRAWING



Existing concrete path with stamped paviour effect



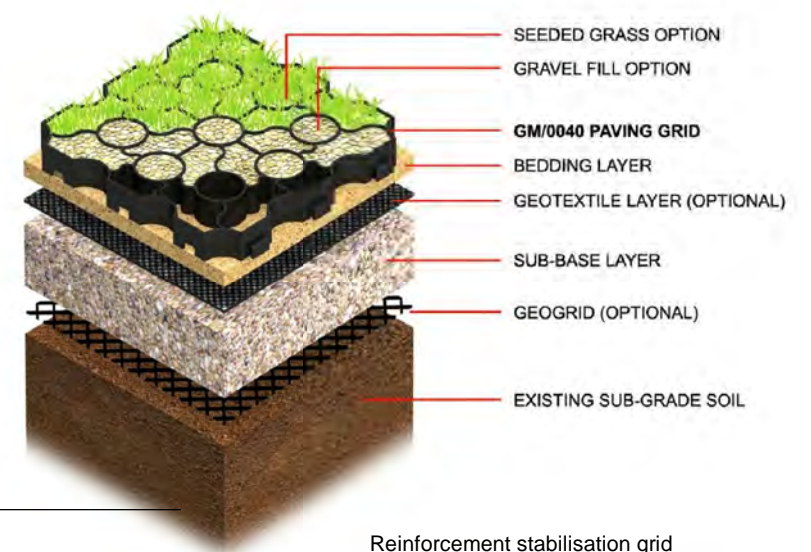
Original View  
 Alterations to provide hard surface, gates & re-build piers  
 to No 14 High Street, Llandaff, Cardiff

A103

Project number	277
Date	MAR 2018
Drawn by	MD
Scale	@A3

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New wrought iron sliding gate to increased opening formed in existing wall, design to incorporate features of original gate, painted black

Increased opening formed in original wall to accept new sliding gate. New formed pillars to be reinstated with original salvaged stone and original capping stones

Salvaged flagstones to form new path and separator strip to front wall

### Proposed View

## Alterations to provide hard surface, gates & re-build piers to No 14 High Street, Llandaff, Cardiff

A104

Project number	277	
Date	MAR 2018	
Drawn by	MD	
Scale	@A3	

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